

City of Arcadia Bicycle and Pedestrian Master Plan

FINAL REPORT



Tindale-Oliver
&
Associates, Inc.
Planning and Engineering

ELEMENT
ENGINEERING GROUP

PlaceMaker
Design Studio, LLC
Urban Design • Landscape Architecture • Land Planning

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Section 1: Introduction

The purpose of the City of Arcadia Bicycle and Pedestrian Master Plan is to effectively integrate bicycle and pedestrian travel within the core downtown area and primary access routes to the downtown area while helping to create a more livable environment, allowing for viable mobility choices for commuting as well as for recreational purposes.

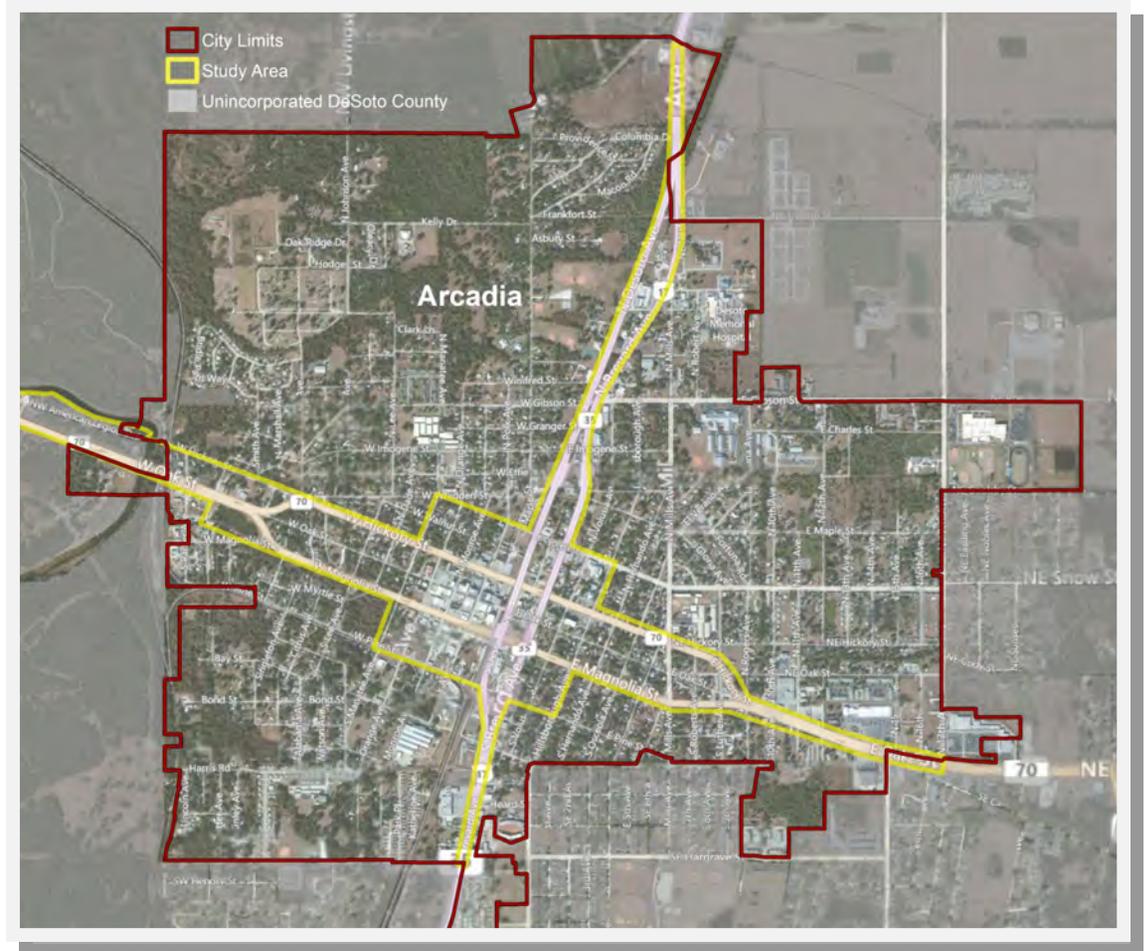
The vision of this plan is to provide a safe, accessible, and connected bicycle/pedestrian system while enhancing the visual appeal of the city.



The focus area of this study is defined in **Map 1**. The area of focus includes:

- the core area of downtown Arcadia with the limits of Manatee Avenue to the west, Hillsborough Avenue to the east, Cypress Street to the north, and Pine Street to the south
- the corridors defined by US 17 and SR 70 within the city limits of Arcadia

Map 1: Study Area



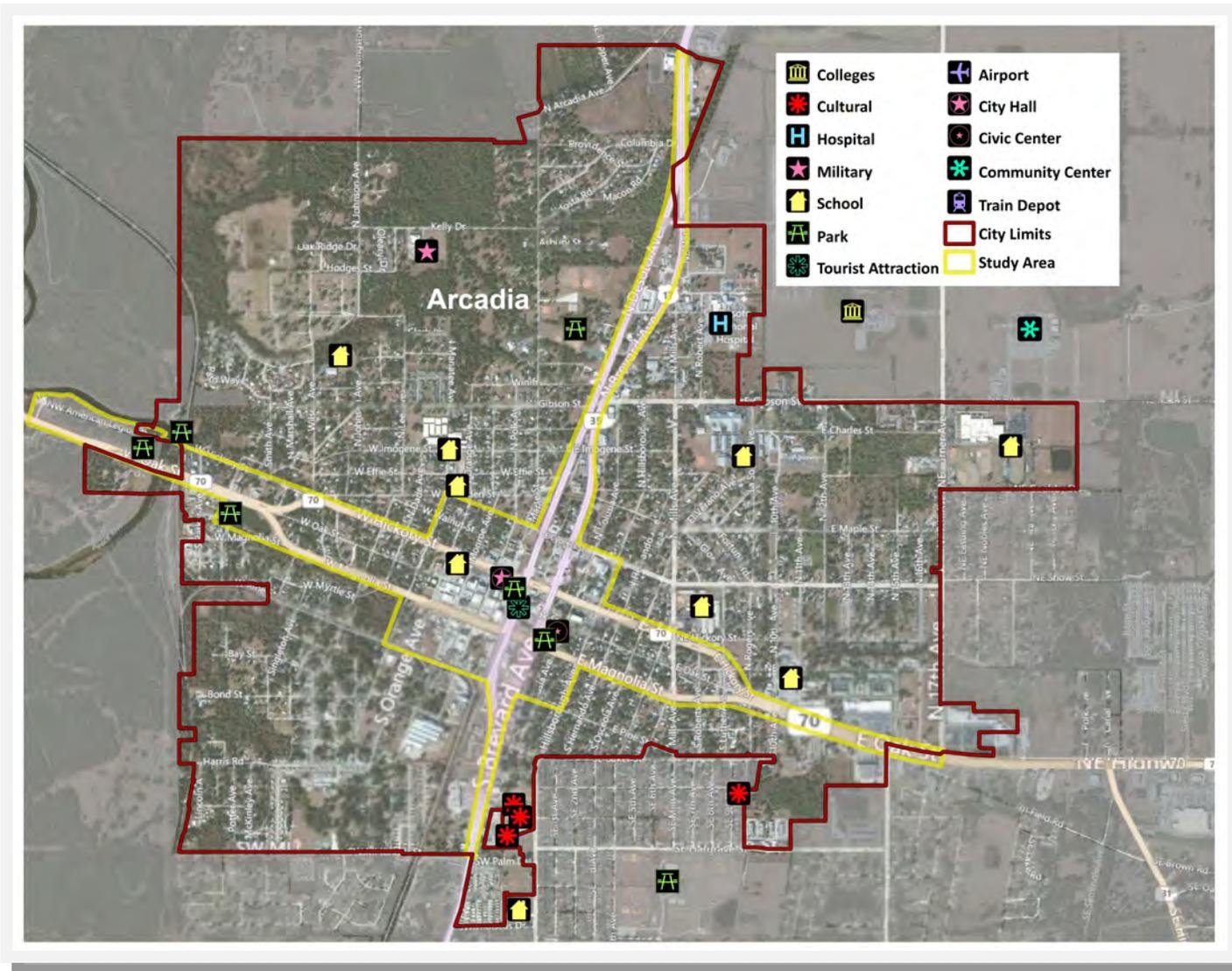
Section 2: Existing Conditions & Past Efforts

The City of Arcadia has many attractions and destinations that generate trips within the area and region. Providing a variety of transportation options between these attractions is key to improving the quality of life within the City—this includes facilities that support and enhance bicycle and pedestrian mobility. This section inventories the existing conditions of the City, including major trip generators and attractors, major employers, and recreational destinations.



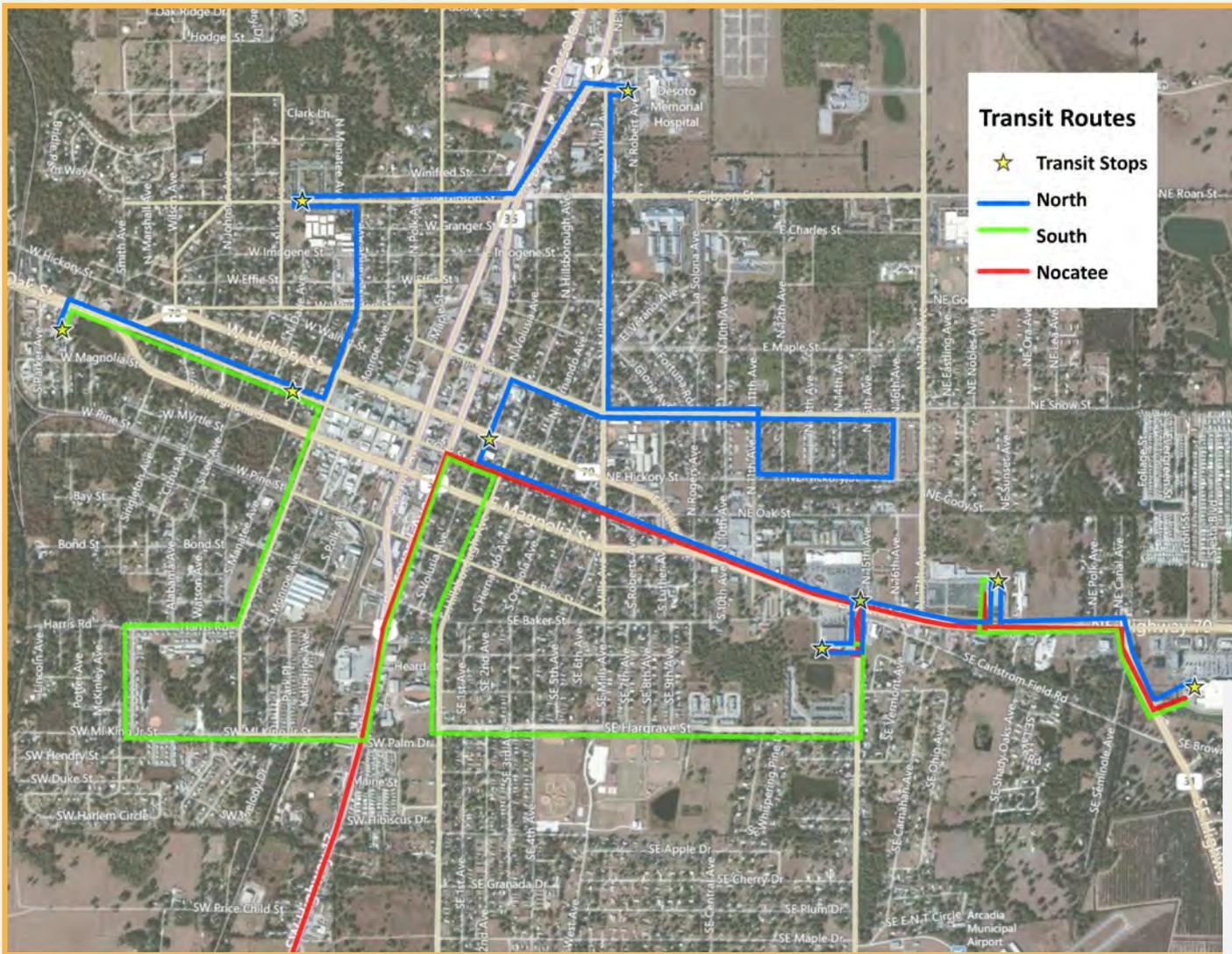
Existing Conditions

This section presents the existing conditions within the city of Arcadia and the study area in Desoto County.



Map 2: Destinations and Attractions

This map displays trip generators, attractions, and destinations within the city of Arcadia.



Map 3: Transit Service: Arcadia inset

The maps on this page display the existing transit service routes and transit stops.

Table 1: Transit Service Summary

Route	Monday-Friday	Saturday	Frequency
North	7:00 AM - 5:25 PM	8:30 AM - 3:55 PM	3 hours
South	8:30 AM - 7:00 PM	No Service	3 hours
Nocatee	No Service	7:00 AM - 6:00 PM	3 hours

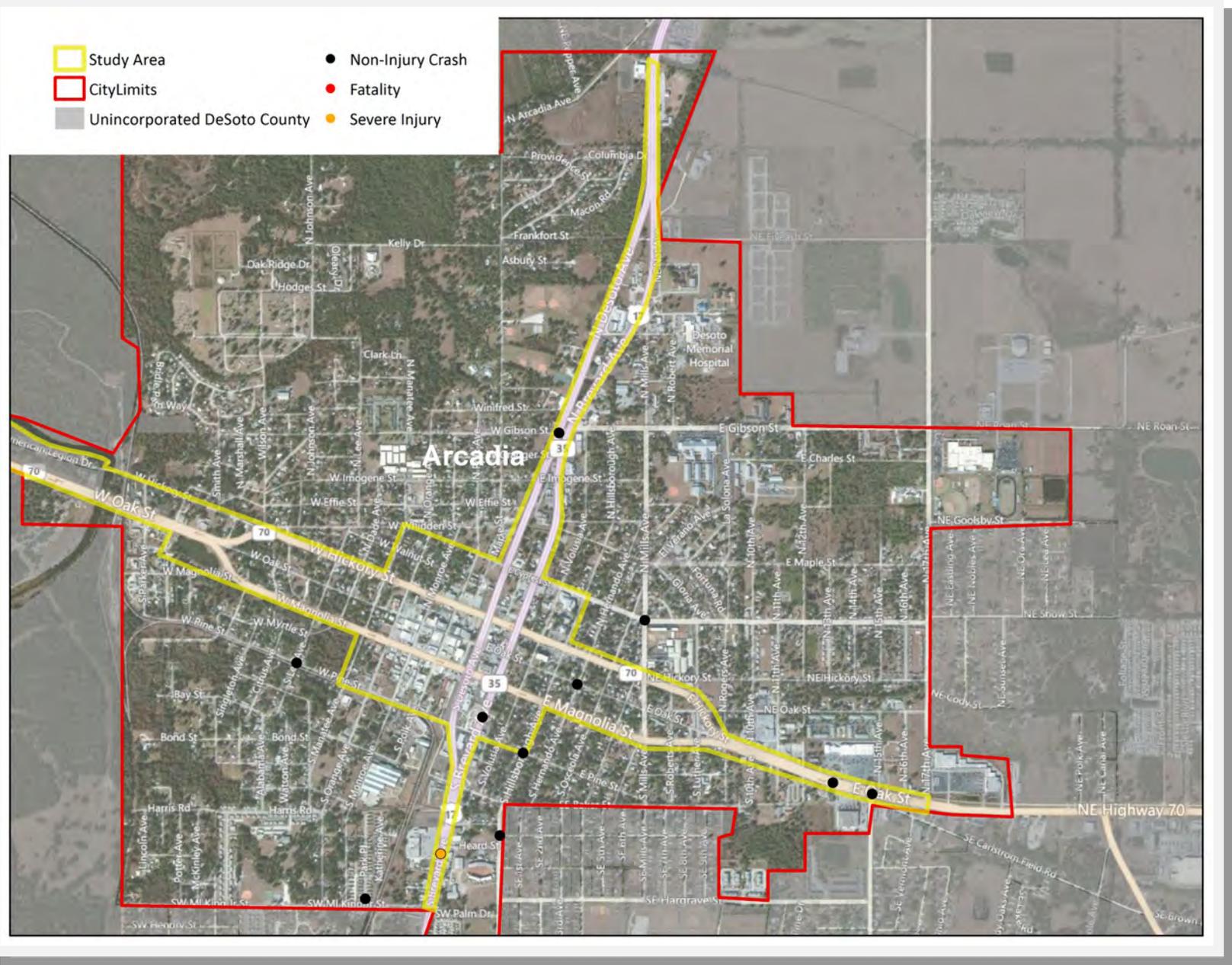
Map 4: Transit Service



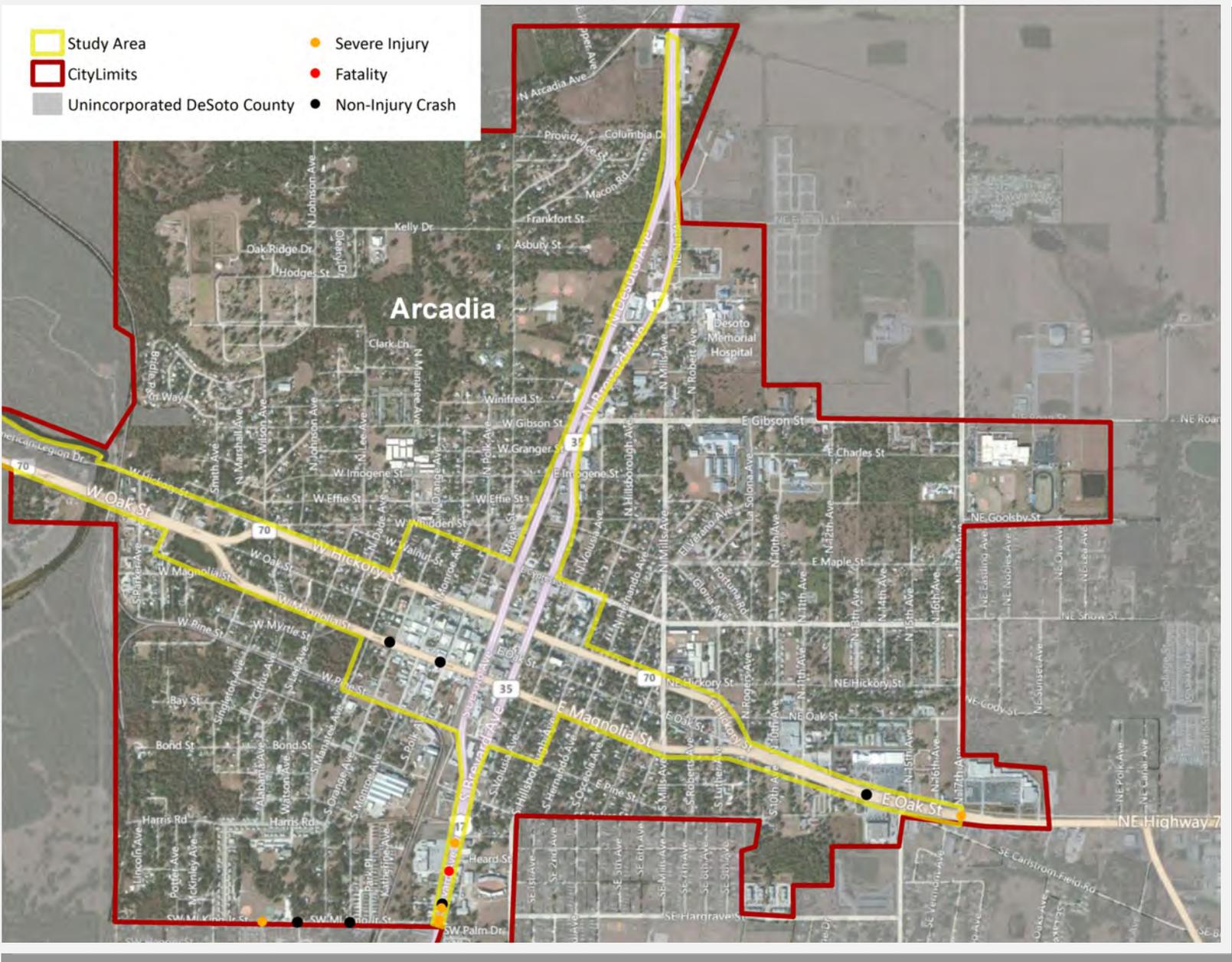
Map 5: Transit Service: Nocatee inset



Map 6: Bicycle Crashes (2008 - 2010) The map below displays bicycle crashes occurring within the city of Arcadia between 2008 and 2010.



Map 7: Pedestrian Crashes (2008 - 2010) The map below displays pedestrian crashes occurring within the city of Arcadia between 2008 and 2010.



Past Efforts

This section presents the recent past redevelopment efforts undertaken within the city of Arcadia and DeSoto County. The following significant endeavors produced valuable information. The City of Arcadia Bicycle and Pedestrian Master Plan relied on the problems and conclusions identified by these efforts in the identification and prioritization of projects and action items.

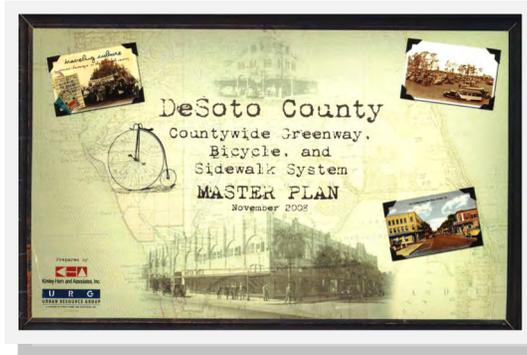
**DeSoto County
Long-Term Recovery Plan (2004)**



This plan was the first major visioning effort for rebuilding DeSoto County in the aftermath of the destruction caused by the summer 2004 hurricanes. This plan includes a comprehensive menu of key projects intended to be used for funding and resource allocation decisions.

This effort was sponsored by the Federal Emergency Management Agency (FEMA) and the State of Florida's Department of Community Affairs (DCA).

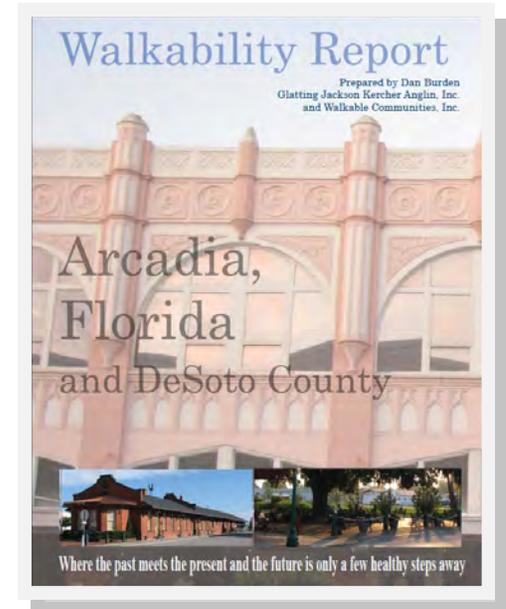
DeSoto Countywide Greenway Bicycle and Sidewalk System Master Plan (2008)



This plan identifies potential areas available for a future greenway, bicycle, and sidewalk system and provides recommendations to guide long-term planning decisions in DeSoto County. The plan's vision is to create a comprehensive countywide system of bicycle, pedestrian, and trail facilities.

This effort was sponsored by the DeSoto County Planning Department and the Florida Department of Transportation Local Agency Program.

**Arcadia/DeSoto County
Walkability Report (2008)**

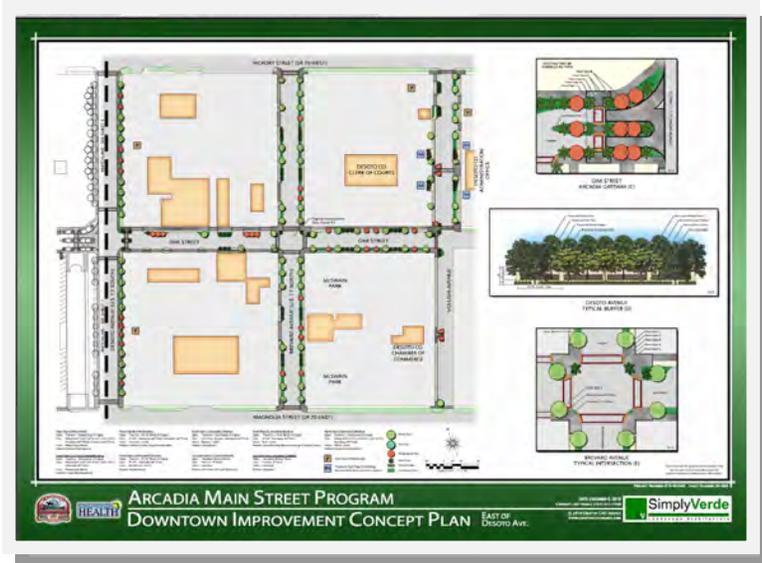


This report was the culmination of an effort to evaluate walkability in Arcadia and DeSoto County. The report is the first step in creating a "smart transportation" future by connecting land use development practices, transportation planning, and economic development.

This effort was sponsored by the DeSoto County Health Department, in association with the Florida Department of Health's Office of Performance Improvement and the Multi-State Learning Collaborative.

Past Efforts

**Downtown Arcadia
Improvement Concept Plan (2010)**



This concept plan for the core Downtown proposes streetscape improvements to enhance the appearance and function of the Oak Street corridor.

This effort was sponsored by Arcadia Main Street and the DeSoto County Health Department.

**Leadership DeSoto
McSwain Park Conceptual Plan (2012)**



This is a concept plan for the redevelopment of McSwain Park in Downtown Arcadia. The plan includes a complete redesign of the entire city block, while incorporating the existing DeSoto County Chamber of Commerce building as well as additional off-street parking.

This effort was sponsored by Leadership DeSoto.



Section 3: Needs Assessment

As part of the Needs Assessment, field reviews were conducted to identify improvements that support bicycle and pedestrian mobility.

On April 26, 2012, an accessibility assessment of existing conditions was conducted throughout the study area and surrounding areas. This assessment included the identification of locations that are not in compliance with the standards set forth in the Americans with Disabilities Act (ADA). Specific locations and proposed improvements were included as part of the Needs Assessment and are presented in this section.

On May 17, 2012, a field review was conducted throughout the study area and surrounding areas for the purpose of identifying potential needs. This includes locations where bicycle and pedestrian safety problems exist, as well as suggestions for future connections, crossing locations, and infrastructure improvements that will provide a more pleasurable and safer experience for pedestrians and bicyclists.



Needs Assessment

The Needs Assessment improvements were categorized into the following types of projects:

- **General operational/safety improvement**
This type of improvement can include resolving an existing safety concern where a pedestrian or bicycle may be encountering a conflict point with automobile traffic. Projects might include a traffic operations improvement that impacts the design of the roadway, modification of curbs and or access points, modification or installation of ramps consistent with the Americans with Disabilities Act (ADA), and other associated pedestrian improvement.
- **Mid-block crossing**
Generally, most pedestrian crashes occur when the pedestrian is attempting to cross the roadway. Providing a new marked crossing at a location where pedestrians naturally choose to cross the street improves crossing support for pedestrians and additional visibility to motorists. This type of improvement may prevent future crashes from occurring and increase walking activities.
- **Opportunity to create/extend a connection**
Efficient pedestrian and bicycle mobility relies on an understanding that one sidewalk or bike lane in isolation has little benefit to a community. Capitalizing on the economic development potential and “placemaking” power of bicycle and pedestrian infrastructure requires a true network or system of routes and facilities throughout a community. This plan seeks to identify locations where key connections are needed to facilitate bicycle and pedestrian travel between community facilities and popular destination points.
- **Sidewalk gap**
A gap in a sidewalk or bicycle facility can pose a safety hazard to an individual traveling along a route. It often forces them to cross the street at an undesirable location. Filling these gaps supports a continuous network of facilities and can be done at a relatively minimal cost, especially when combined with existing planned roadway improvement projects.
- **Upgrade streetscape**
These improvements identify locations where the public right-of-way could be redesigned, used more efficiently, and provide a safer and more pleasant experience for the public.
- **Upgrade crossing**
Over time, existing infrastructure becomes worn out, outdated and obsolete. This often requires a reexamination of existing locations where marked crossings exist and an identification of potential upgrades or maintenance projects. These improvements identify crossing locations in need of an upgrade.

Table 2: Needs Assessment The table below displays the needs identified during the development of the plan.

Need #	Location	Description	Proposed Improvement
1	DeSoto Ave at Hickory Ave	General operational/safety improvement	Channelize turn lanes. Install/refurbish existing ADA ramps where needed.
2	Hickory Ave between Desoto Ave and Brevard Ave	General operational/safety improvement	Fix continuous dropped curb on north edge of pavement. Replace with hard curb and driveway curb cut if necessary.
3	Hickory Ave at Volusia Ave	Mid-block crossing	Refurbish/upgrade existing marked crossing. Restripe, add stop bars, install advanced warning signage.
4	DeSoto Ave (US 17 Southbound) between SR 70 pair	Opportunity to create/extend a connection	Rehabilitate/widen sidewalk on west side. Extend north on railroad right-of-way adjacent to The Depot and create a linear park/trail along the west side of SR 70.
5	DeSoto Ave at Magnolia Ave	General operational/safety improvement	Channelize right turn onto DeSoto.
6	Magnolia Ave at DeSoto Ave	General operational/safety improvement	Add ADA ramps at the corner where Sun Trust Bank is located.
7	Oak St at DeSoto County Administration Building	Mid-block crossing	Add curb extension and mid-block crossing on Oak Street connecting the County Building to McSwain Park.
8	Oak St at DeSoto County Courthouse Building	Mid-block crossing	Add curb extension and mid-block crossing on Oak Street.
9	Volusia Ave between Oak Street and Hickory Ave	Mid-block crossing	Add curb extension and mid-block crossing on Volusia Ave.
10	Magnolia Ave at Volusia Ave	Upgrade streetscape	Add angle parking from Oak Street to Magnolia Ave. Add curb extensions and bulbouts where needed.
11	Hickory Ave at Hillsborough Ave	Mid-block crossing	Add ADA ramps at existing marked crossing.
12	Hillsborough Ave from Hickory Ave to Magnolia Ave	Sidewalk gap	Refurbish existing sidewalk and fill gaps.
13	Oak Street from Brevard to Hillsbrough Ave	Upgrade streetscape	Add angle parking along Oak Street.
14	SR 70 at Arcadia Village (East of Downtown)	Crossing upgrade	Refurbish existing marked crossing. Add stop bar, ADA ramps.
15	Polk Street South of Magnolia Ave	Upgrade streetscape, sidewalk gap	Add curb extensions and on-street parking. Rehabilitate sidewalk on East edge of pavement. Add expansion joints to existing sidewalk on West side to prevent cracking. Complete sidewalk gaps on West edge of pavement. Add missing sidewalks to connect south to Pine Ave.
16	Magnolia Ave at Manatee Ave	Upgrade crossing	Consider adding Rectangular Rapid Flashing Beacon (RRFB) to existing crossing.
17	Magnolia Ave just South of Manatee Ave	General operational/safety improvement	Fix continuous dropped curb on east edge of pavement. Replace with hard curb and driveway curb cut if necessary.
18	Orange Ave just North of Magnolia Ave	Sidewalk gap	Complete sidewalk gaps on both side of the street.
19	Monroe Ave just south of Magnolia Ave	Sidewalk gap	Complete sidewalk gaps on both side of the street.
20	Magnolia Ave at Polk St	General operational/safety improvement	Close curb/cut driveway on northwest corner (too close to intersection). Fix dropped curb at northwest corner. Convert painted-out areas on Magnolia Ave into curb extensions. Add curb extensions along Polk St. Add a mid-block crossing across Magnolia.
21	Magnolia Ave from Polk St to DeSoto Ave	General operational/safety improvement	Fix continuous dropped curb along North edge of pavement.
22	Polk Street between Hickory Ave and Oak St	Upgrade streetscape	Convert existing painted-out areas into curb extensions. Redesign parking lot to increase capacity.
23	Monroe Ave just North of Oak St	General operational/safety improvement	Close continuous dropped curb/driveway south of alley.
24	Hickory Ave at Polk St	General operational/safety improvement	Refurbish ADA ramps. Modify driveway opening. Fix continuous dropped curb on North edge of pavement.
25	Magnolia Ave between Monroe Ave and Polk St	Upgrade streetscape	Add curb extension on south edge of pavement. Narrow travel lanes.
26	Hickory from Wilson Ave to Peace River	Opportunity to create/extend a connection	Add shared-lane markings along Hickory to connect with Morgan Park/Peace River.
27	Manatee Ave from Magnolia Ave to Pine St	Opportunity to create/extend a connection	Rehabilitate/replace sidewalk on East edge of pavement. Install new sidewalk along West edge of pavement. Add shared-lane arrows connecting Pine St to Oak St. Add new sidewalks on Pine Street from Baldwin Ave (Lake Katherine Park) to Brevard Ave.
28	Manatee Ave between Oak St and Magnolia Ave	Sidewalk gap	Add sidewalk on west edge of pavement.
29	Magnolia Ave at Manatee Ave	Upgrade crossing, Sidewalk gap	Add stop bar for existing crossing. Mark crossings on sidestreets. Add sidewalk on Magnolia from Manatee to Orange (north edge of pavement).
30	Pine St from Baldwin Ave to Brevard Ave	Opportunity to create/extend a connection	Add new sidewalks on Pine Street from Baldwin Ave (Lake Katherine Park) to Brevard Ave.

Additional Needs

In addition to the needs identified in Table 2, a conceptual design for streetscape improvements is also included in the Needs Plan. This Enhanced Streetscape Alternative was selected during the public involvement activities, but was not included in the Priorities for Improvements because it is unfunded. The Enhanced Streetscape Alternative is presented on the following pages.



Enhanced Streetscape Alternative—Oak Street from Orange Avenue to Brevard Avenue

The importance of Oak Street as the centerpiece of Downtown Arcadia merits a vision for a redeveloped corridor that complements the historic and cultural value of the surrounding built environment. Improving

the pedestrian experience along this corridor will support the future of Downtown Arcadia as a historic destination and regional attraction. **Figure 1** presents the need for a redeveloped streetscape along the Oak

Street corridor between Orange Avenue and Brevard Avenue. Detailed insets of the intersection improvements and mid-block crossing improvements are presented on page 3-8 and 3-9.

Figure 1: Diagram of Enhanced Streetscape Alternative: Full Corridor



STREET TREE LEGEND

- FLOWERING ACCENT TREE
- PALM TREE
- CANOPY TREE



CANOPY TREE



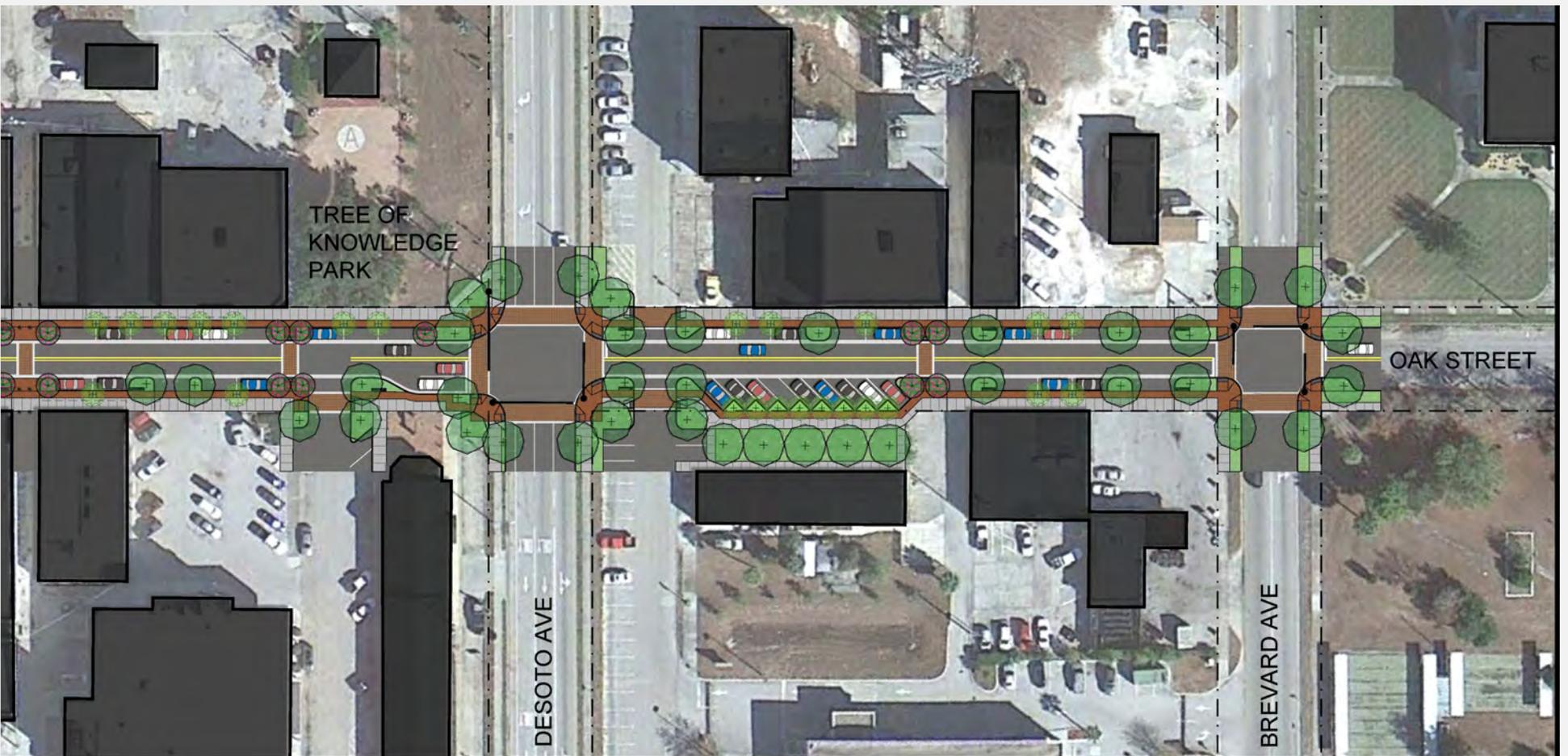
PALM TREE



FLOWERING ACCENT TREE



VALLEY GUTTER EXAMPLE



Enhanced Streetscape Alternative—Oak Street from Orange Avenue to Brevard Avenue

Figure 2: Diagram of Enhanced Streetscape Alternative: Intersection Improvements

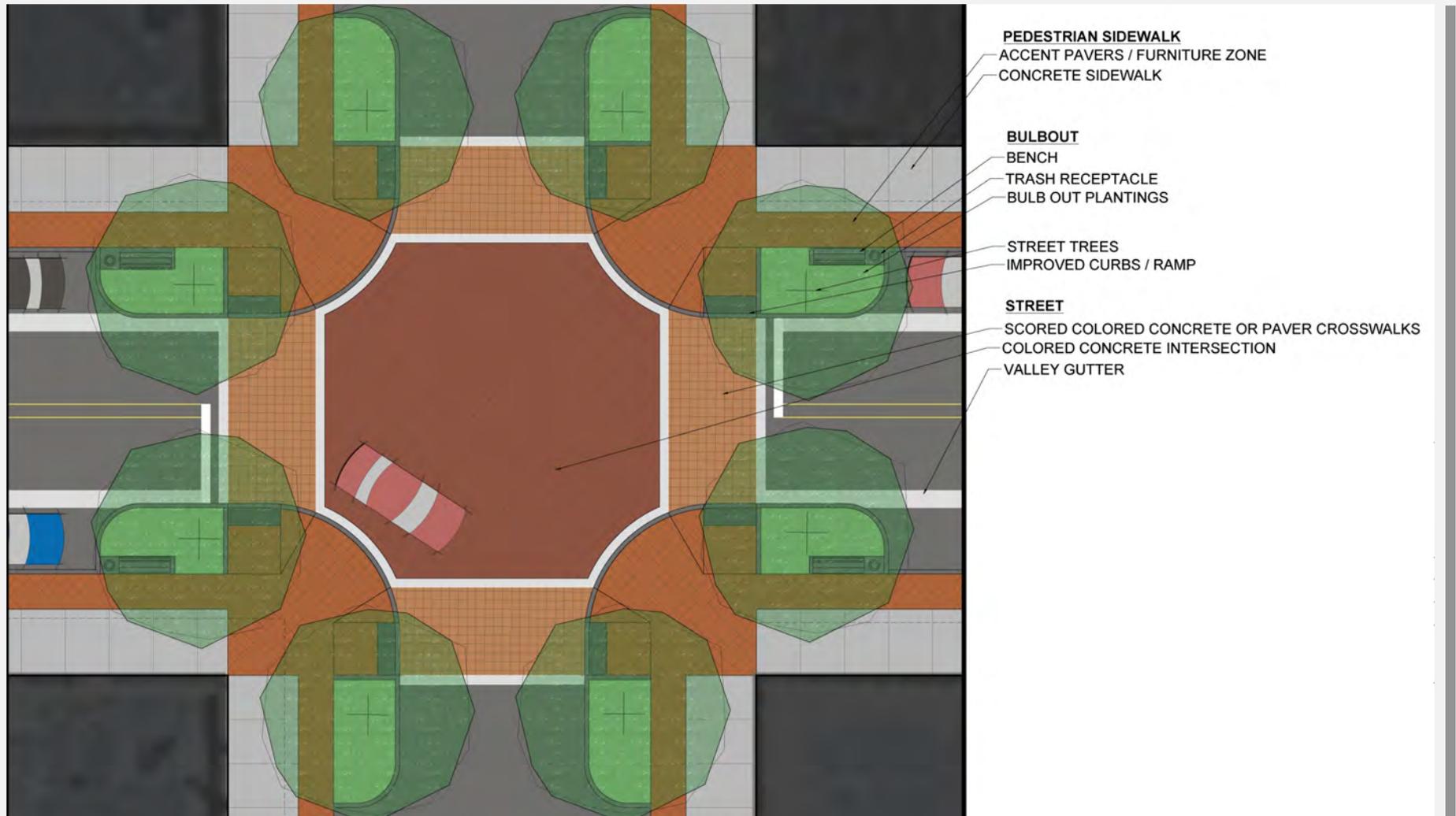
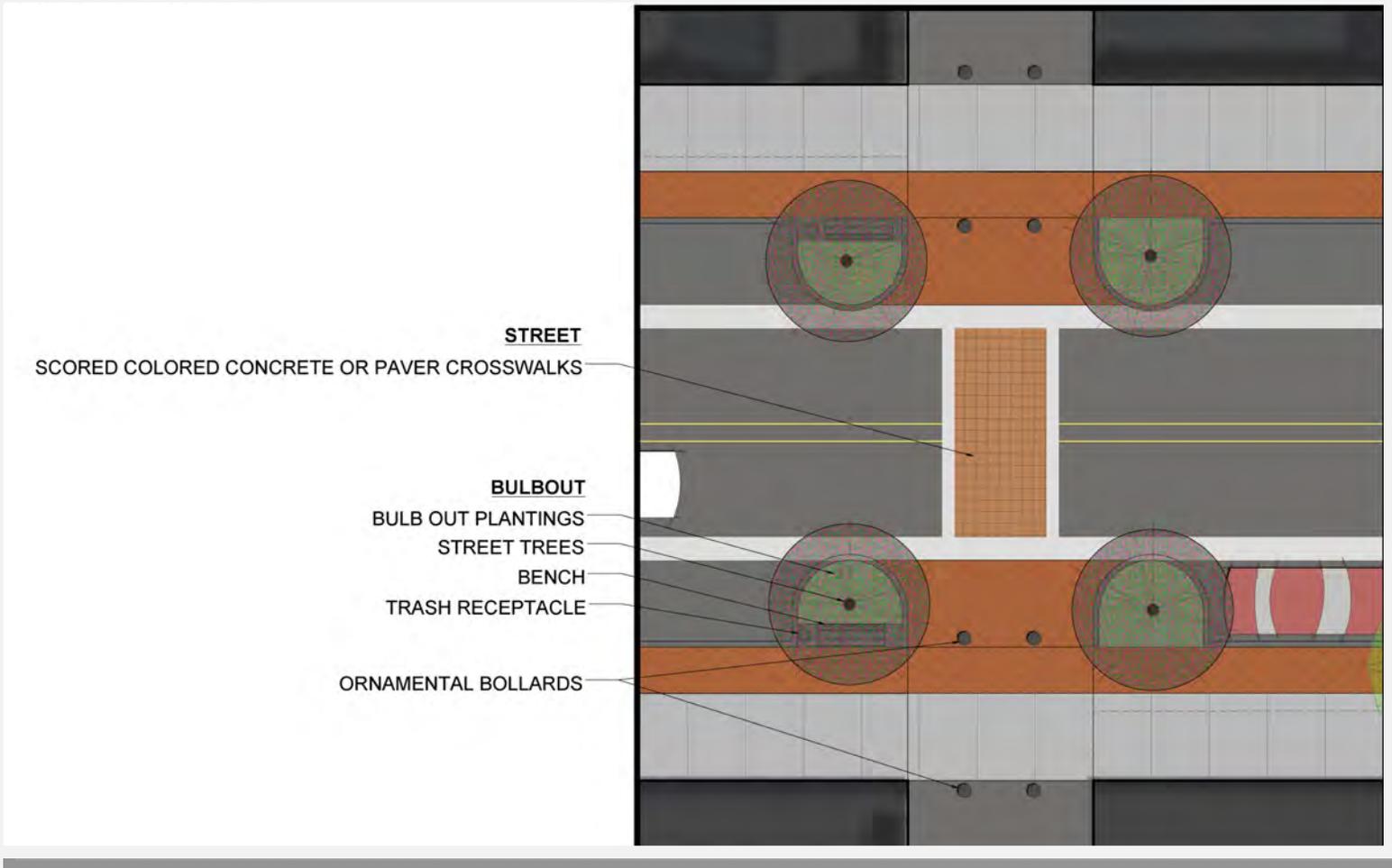


Figure 3: Diagram of Enhanced Streetscape Alternative: Mid-block Crossing Improvements





Section 4: Best Practices

Understanding nationwide trends in accommodating bicycle and pedestrian planning in small historic towns is a key part of this effort. This section presents best practices associated with pedestrian facilities planning, design and safety relevant to the city of Arcadia.

The key themes of this section include the following:

- Context-Sensitive Design
- Understanding Pedestrians
- Mid-Block Crosswalks
- Bicycle Planning
- Historic Downtowns
- Funding Opportunities

Context-Sensitive Design

The city of Arcadia values its rich history and culture. Of particular importance to this plan is identifying context-sensitive pedestrian and bicycle design solutions that emphasize preserving the character and feel of Downtown Arcadia. This section presents national best practices that demonstrate how pedestrian and bicycle mobility and safety can be harnessed to complement and drive development in historic downtowns.



Best Practices

Context-Sensitive Design

The last century has seen great advancements in transportation and urban form. Specifically, modern automobiles have evolved to become larger in size and move at higher speeds. Context-sensitive street design understands the value of historic preservation and cultural heritage while also balancing the safety and business needs of today's modern society.

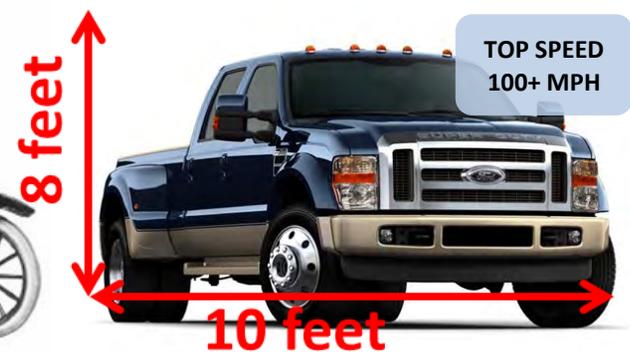
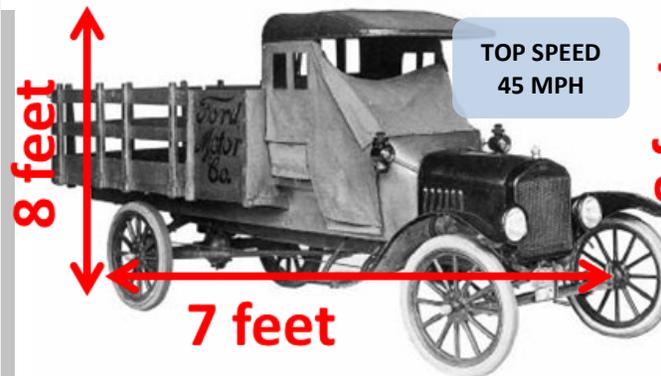
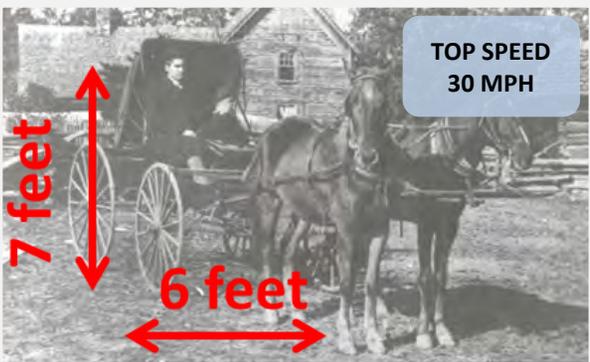
Downtown Arcadia (1900)



Downtown Arcadia (1910)



Downtown Arcadia (2012)



In 1900, the town of Arcadia was the seat of DeSoto County and was an emerging pioneer town in southwest Florida, about 40 miles east of Sarasota. Buildings were largely of wood construction. Streets were comprised of dirt. The primary mode of transportation aside from walking was horse-drawn wagon.

After the Great Fire of 1905, the town of Arcadia was still the seat of DeSoto County. The downtown was reconstructed using stone and brick. The streets downtown were repaved with bricks. The primary mode of transportation was the first internal-combustion automobiles.

Today, over a century has passed, and the town of Arcadia remains the seat of DeSoto County. Most of the buildings downtown remain standing, having been constructed of stone and brick. The streets downtown are repaved with asphalt. The primary mode of transportation is the modern automobile.

Understanding Pedestrians: The Four Principles

Principle #1

Pedestrians want and need to cross the street safely.

Every individual is a pedestrian at some point in his or her day. Even if a trip is made by car, the beginning and end typically consists of walking at least a small distance. Pedestrian safety is relevant to everyone because everyone is a pedestrian. The mother crossing the street in the picture to the right is primarily concerned with crossing the street safely.



SR 70 / Magnolia Street at Polk Street

Principle #2

Drivers need to understand the pedestrian's intent.

Recognizing when a pedestrian is attempting to cross the street is a key safety challenge for motorists. Good design and supportive infrastructure can help pedestrian make their desire to cross the street known to drivers.



Understanding Pedestrians: The Four Principles

Principle #3

Keep crossings short.

While choosing to cross the street at a marked crosswalk is always safer than choosing to cross at an unmarked location, the distance that a pedestrian must cross has many implications on the likelihood that a crash will occur. A longer crossing distance increases the exposure time for pedestrians—the time that a pedestrian is crossing the roadway and exposed to vehicle conflicts. At marked crosswalks, a longer crosswalk also requires more time for the pedestrian to reach the other side of the roadway, which causes more delay for vehicles waiting to proceed. Longer

crossing distances also creates challenges for slower pedestrians, including the physically challenged, young children and older adults.

The images below display crosswalks that may prove challenging to cross due to their length. The images to the right are examples of crosswalks that have been modified to reduce crossing distance by the use of curb extensions and median refuge islands. Both treatments do not impact the number of travel lanes and have a minimal impact on the capacity of the roadway.



Examples of long crosswalks



Examples of mid-block crosswalks that minimize the crossing distance: Curb extensions (above) and a raised median with pedestrian refuge (below).



Understanding Pedestrians: The Four Principles

Principle #4

Speed matters

Vehicle speeds directly impact a motorist's ability to stop. The higher the travel speed, the greater the distance required to stop a motor vehicle. In areas with pedestrian activity, ensuring that vehicles are able to stop easily and quickly is key in avoiding pedestrian collisions. Keeping vehicle speeds low also creates a more pleasant, comfortable environment for pedestrians in the immediate vicinity. It is imperative that vehicle speeds reflect the surrounding built environment and land uses.

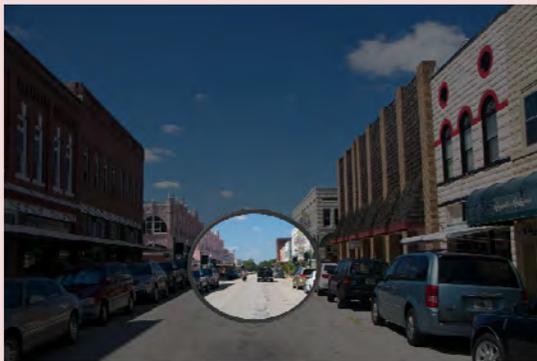
Figure 4 below demonstrates the combined effect speed has on peripheral vision and stopping distance. Traveling at a slower speed opens a driver's peripheral vision and allows the driver to stop sooner and within shorter distance. Even traveling just 5 miles per hour slower could turn a potential accident into a close call.



A raised crosswalk (above) or curb extension (below) slows vehicles and creates comfort and convenience for pedestrians.



Figure 4: Travel Speeds and Impact on Stopping Distance and Peripheral Vision



45 mph—requires 195 feet of stopping distance.



35 mph—Requires 135 feet of stopping distance.



25 mph - Requires 85 feet of stopping distance.

Pedestrian Design: Mid-Block Crosswalks

Key to planning for pedestrian safety is considering that most pedestrian accidents occur when a pedestrian is attempting to cross the roadway. Typically, the safest place for a pedestrian to cross is at a signalized intersection using a marked crosswalk. When a signalized intersection is not nearby, pedestrians often must choose to cross mid-block, away from a crossing. Without the protection of a marked crossing to provide warning to motorists, these situations can often lead to a crash. An extremely effective countermeasure is to identify locations with a significant amount of pedestrian crossings and install a mid-block crosswalk. The example below is of Fourth Street North in St. Petersburg, Florida.

Rectangular Rapid Flashing Beacon (RRFB)

The RRFB is a traffic control device that uses existing pedestrian crossing signage combined with pedestrian-activated dual flashing rectangular-shaped high-intensity LED beacons that illuminate in a wig-wag flashing sequence (left light on, then right light on). Unlike standard round warning beacons, the RRFBs only illuminate when a pedestrian is present. Preliminary studies undertaken by the City of St. Petersburg indicate a compliance rate of between 80 and 100 percent. This device is currently under interim approval by the Federal Highway Administration and is expected to receive full approval with the next update of the Manual on Uniform Traffic Control Devices (MUTCD).

(FHWA MUTCD IA-11 - July 16, 2008)





Multi-use trail in Dunedin, Florida.



Bicycle lanes: Before and after.

Bicycle Planning

Bicycle planning includes planning for a more interconnected network of bikeways that make bicycling convenient, safe, and enjoyable. Additionally, bicycle planning requires the accommodation of bicycles on existing facilities in a safe manner. Multi-use trails are an important tool in improving bicycle accommodations. These facilities are wide, paved pathways that are used by bicyclists and pedestrians and are closed to vehicular traffic. Multi-use trails can be recreational in use or support non-vehicular travel in a small downtown community such as the example in Dunedin to the left.

Often, space exists on existing roadways for the designation of bicycle lanes. In many of these cases, a resurfacing project can incorporate the modification of pavement markings to provide a new bicycle lane. The example on the bottom left displays how modifying pavement markings can change the appearance and function of the roadway.

Shared-Lane Markings

Shared-lane markings—or “sharrows”—are useful on roadways where bike lanes are not feasible. This type of treatment encourages bicyclists to ride away from the edge of the roadway and instead take the full lane as allowed by law. This prevents the motorist from using the same lane to pass, which causes safety concerns. Sharrows are especially useful in providing continuity along constrained sections or corridors with existing bicycle facilities.



Historic Downtowns

Maintaining the character of the community is a key aspect of this plan. Preserving the quality of these areas for pedestrians and cyclists supports their function as a walkable “main street” and encourages economic development in areas in need of revitalization. The best practices highlighted in this section are proven techniques to complement the historic, walkable, vibrant, and unique nature of individual communities. Downtown Pensacola, Florida, (pictured below and to the right) was mentioned during the public involvement activities as an example of effective context-sensitive street design applied to a historic downtown setting.



Including streetscaping, street furniture, utilities, sidewalk pathways, retail signage, and on-street parking is often easily possible, even within a limited right-of-way.



Funding Opportunities

A wide range of funding sources is available for pedestrian and bicycle infrastructure projects. The following funding sources may provide opportunities for the implementation of the improvements described in this plan, without a significant financial burden placed on local governments.

Highway Safety Improvement Program (HSIP)

The Highway Safety Improvement Program is a core federal-aid funding program whose goal is to achieve a significant reduction in fatalities and serious injuries on all public roads. HSIP funds are administered through the Florida Department of Transportation. Highway safety improvement projects correct or improve a hazardous road location or feature, or address and existing highway safety problem. Specific site or system-wide improvements that reduce deaths and severe injuries are eligible to receive funding. Funds may be used to address safety issues independently without completely reconstructing entire roadway segments or intersections to all of the latest policies and standards. The Highway Safety Improvement Program supports Florida's Strategic Highway Safety Plan (SHSP), which focuses on problems areas where the greatest potential reduction in the number of fatal and serious injury crashes exist. The SHSP identifies four emphasis areas: Aggressive Driving, Intersections, Lane Departure, and Vulnerable Road Users (pedestrians, bicyclists and motorcyclists).

Transportation Alternatives Program/ Safe Routes to Schools Program (SRTS)

Transportation Alternatives is a newly established program created by the federal transportation reauthorization Moving Ahead for Progress in the 21st Century (MAP-21). The new legislation consolidates previous provisions for bicycle and pedestrian related projects under new categories. Florida's Safe Routes to Schools Program is a core federal-aid funding program that will be funded under the Transportation Alternatives program. SRTS supports and facilitates bicycling and walking to and from school. Specifically, the goals of the SRTS program are:

1. to enable and encourage children, including those with disabilities, to walk and bicycle to school
2. to make bicycling and walking to schools a safer and more appealing transportation alternative, thereby encouraging a healthy and active lifestyle from an early age
3. to facilitate the planning, development, and implementation of projects and activities that will improve safety and reduce traffic, fuel consumption, and air pollution in the vicinity of schools.

The SRTS program includes infrastructure and non-infrastructure projects.

Local Agency Program (LAP)

The Florida Department of Transportation has historically contracted with other governmental agencies to develop, design, and construct transportation facilities, to acquire right-of-way and to reimburse these governmental agencies for services provided to the traveling public. LAP uses federal funds distributed by the Federal Highway Administration (FHWA). The LAP program is administered at the FDOT District level.

Resurfacing, Restoration and Rehabilitation (3R)

3R Projects focus primarily on the preservation and extension of the service life of existing facilities and on addressing existing safety concerns. Combining a safety project with a planned 3R project can yield considerable cost savings compared to undertaking both projects independently.

Other Funding Sources

Additional funding opportunities can be realized by pairing a safety improvement with an existing capacity improvement. Additionally, local funding can be paired with funding from different sources at the state and federal levels to provide the most leverage in a competitive economic climate and produce the greatest return on an investment.



Section 5: Public Involvement

It was important that the residents and business owners in Arcadia—those affected most by the improvements proposed in this plan—were heavily involved in the development of this plan. To accomplish this, the following public workshops were held:

- April 28, 2012: Observation and public surveys at the Antique Fair
- April 2012: Stakeholder interviews
- May 17, 2012: Needs Assessment
- June 14, 2012: Consensus Building Workshop #1
- July 26, 2012: Consensus Building Workshop #2

A summary of these efforts is presented on the following pages.



Public Involvement

As part of the public involvement portion of the plan, the following activities were conducted to generate community support and approval of the improvements.

- **Observations and public surveys at Arcadia Antique Fair—April 28, 2012**
Citizens and visitors were interviewed and asked a series of questions about Downtown Arcadia. The focus of the interviews was to gauge how the public views Downtown Arcadia and to survey their ideas for future downtown redevelopment. Participants were asked questions about what they enjoy about Downtown Arcadia and about any improvements that could reinvigorate the Downtown area.

Participants felt that it should be a top priority of property owners and community leaders to help preserve character of the old buildings. It was expressed that there is a need for more parks or public areas within and around Downtown. Many felt that the City should repair existing sidewalks that are damaged (especially on sidestreets). It was expressed that a grocery store is needed in Downtown to serve local residents that now have to travel out to the WalMart on SR 70. Participants felt that bringing in a younger population would help re-energize the area—perhaps something to attract children such as playgrounds. Many participants stated that parking is somewhat confusing. Visitors were unsure of appropriate places to park. Participants noted that the Downtown area needs street trees and awnings to provide shade and relief from the sun and heat.



- **Stakeholder Telephone Interviews—April 2012**
Key stakeholders were identified by City and County staff to participate in one-on-one interviews. These interviews focused on gauging how elected officials and community leaders view Downtown Arcadia and pedestrian and bicycle safety throughout the city. Stakeholders were also given the opportunity to provide input on redevelopment ideas for the Downtown.

The interviews consisted of a visioning exercise, where stakeholders were asked to answer questions about how they envision the future of Arcadia in terms of the condition of buildings in the Historic Downtown, the land uses, needed resources, and economic development priorities. Stakeholders also described other places that have successfully revitalized historic downtown areas as good examples for what Arcadia can accomplish.

Stakeholders cited the following issues as being top concerns:

- Adequate sidewalks and paths serving schools and other key destination points.
- Parking in and around downtown Arcadia
- Inadequate wayfinding signage in downtown Arcadia
- Need for landscaping in downtown Arcadia
- Street lighting
- Sidewalks and marked crossings

- **Consensus-Building Workshop#1—June 14, 2012**

On June 14, 2012, a consensus-building workshop was held to build on discussions from the previous interviews concerning potential improvements. The workshop was held from 5:30 pm to 8:00 pm at the DeSoto County Administration Building in Arcadia. The goal was to gain feedback on needed bicycle and pedestrian projects and priorities in Arcadia in the future.

Participants were provided with a “headliner exercise” to serve as an opening visionary exercise. The point was to obtain from participants other examples of great historic downtowns they have visited, what specifically they enjoyed about those downtowns, and what bicycle/pedestrian improvements they would like to see in Arcadia. They were also asked what kind of headline they would like to see for Arcadia in 20 years and what headline to they most fear.

Participants were then provided with a handout that outlined and described several improvements and were asked to rank each priority and comment on details. They were also asked to choose one of four options for the Oak Street Downtown streetscape improvements, which included the existing condition as well as three alternatives.

The feedback provided was used to refine the priorities for the second consensus-building workshop. The workshop participants ranked the improvements at Oak Street at Polk St and Monroe Street as the first priority, followed by the pedestrian improvements on US 17 south of Downtown. On the Downtown Streetscape improvements, participants also ranked the Enhanced Streetscape with Parallel Parking first, followed by the Incremental Enhanced Streetscape. Other comments included making improvements to lighting downtown and providing public restrooms and accommodations for persons with disabilities as well as parking and “wayfinding” to let people know about Downtown Arcadia.

The renderings for the Oak Street Downtown Streetscape improvements were further developed, and costs for the priorities were created for the second consensus-building workshop.

- **Consensus-Building Workshop #2—July 26, 2012**

On July 26, 2012, the second consensus-building workshop was held. It took place at the DeSoto County Administration Building in Arcadia from 5:30 pm to 8:00 pm. The purpose of the workshop was to confirm the bicycle and pedestrian improvement priorities identified at Consensus-Building Workshop #1, and to discuss implementation of these projects or programs. Fifteen people attended the workshop.

During the workshop, participants were given a handout summarizing the improvement priorities as identified in the previous Consensus-Building Workshop. The details and potential funding sources were reviewed for each project by the group, and the opportunity for discussion and questions was available for each project. As each project was discussed, each individual participant was asked to indicate whether or not he or she agreed with the project ranking in the list of priorities.

While individuals may have preferred certain projects be moved up or down in the list, overall, the group agreed that the list, as presented, reflected the appropriate prioritization. Therefore, no changes were made to the priorities list.



Section 6: Priorities for Improvements

This section presents the prioritized improvements identified throughout the development of the plan. These improvements were selected during the identification of needs and were chosen by stakeholders and citizens as key priorities for future improvements.



Priorities for Improvements

This section summarizes the specific bicycle and pedestrian priorities for the Master Plan that have been identified. These priorities are identified on in **Map 9** and in **Table 3** as well as in further detail in the following pages.

Additional projects that were not included as the top prioritized project but were considered in the Needs Assessment are documented within the Final Report.

Map 9 provides an overall summary of the locations prioritized for improvement. Each number corresponds to an improvement shown on **Table 3**. The table also shows recommended responsible parties and potential funding sources. These improvements were deemed to be the highest priorities in public feedback and input. The numbered rankings were achieved through a prioritization process that took place at the public workshops. More information on these public workshops is located later in this report.

Map 9: Priorities for Improvements

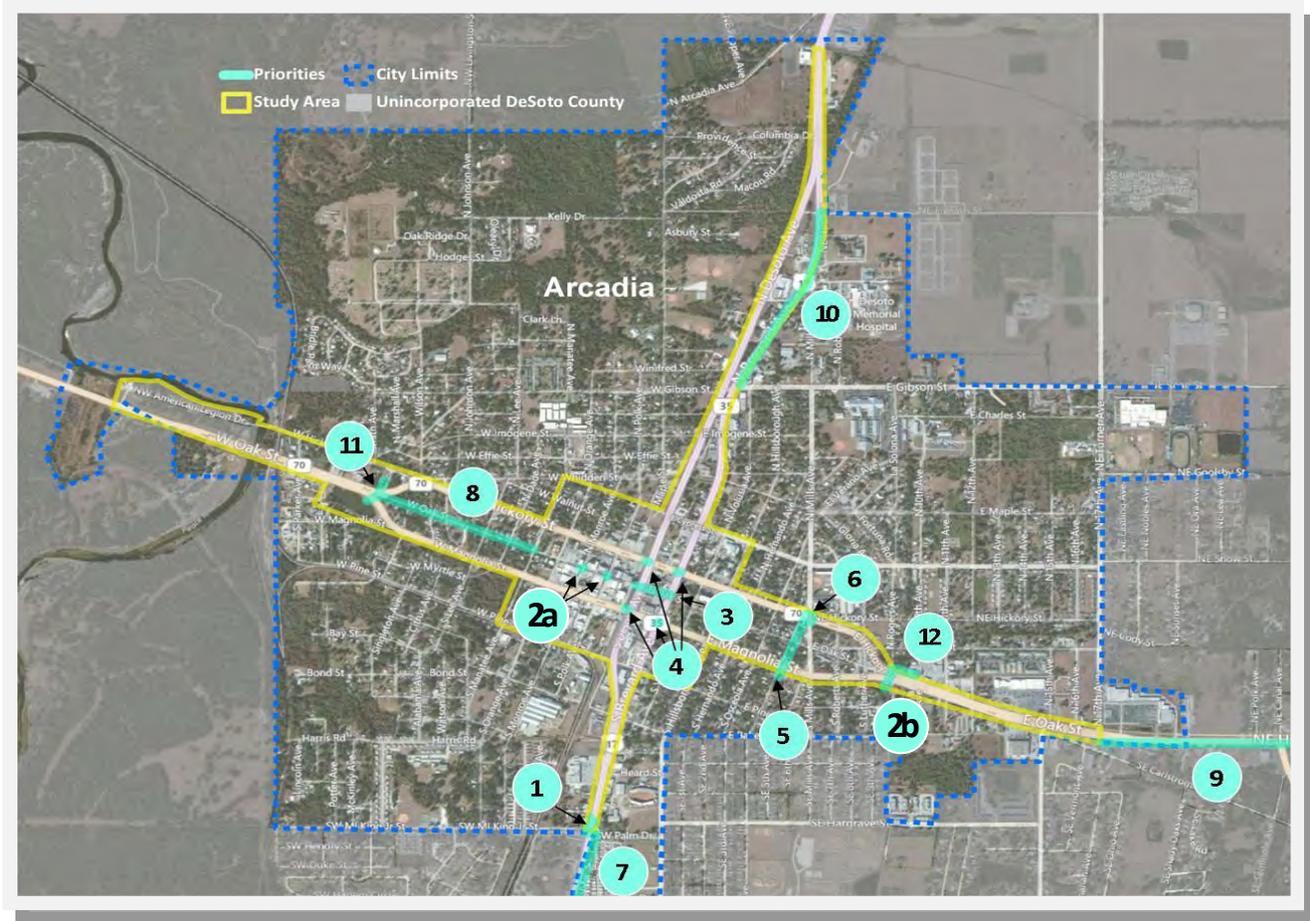


Table 3 presents each proposed improvement in order of final prioritization. The information in the table includes location, description of issue or concern, description of proposed improvement, lead responsible agency to follow-through on moving the project forward, estimated funding amount, and potential funding source(s). The following pages provide further visual information for each priority on the specific improvements that are recommended.

Table 3: Priorities for Improvements

Priority #	Location	Description	Potential Improvements	Lead Responsible Agency	Estimated Funding Amount (See detailed costing guide)	Potential Funding Source
1	US 17 at Heard St/Reynolds St	Mid-block crossing	a) Add crossing markings at at least one location - 1 on Reynolds Street, and 1 on Heard Street b) Add crosswalk and add temporary curb refuge island south of Reynolds Street/US 17	FDOT District 1 Safety Office City of Arcadia	\$7,558.53	FDOT Highway Safety Improvement Program
1	US 17 at Nelson St	Mid-block crossing	a) Option 1: Add crosswalk and temporary curb north of Nelson (would prohibit SB Left for a residential unit) b) Option 2: Add crosswalk and temporary curb between two Circle K driveway entrances (would require right in/right out at Circle K)	FDOT District 1 Safety Office City of Arcadia	4,871.56*	FDOT Highway Safety Improvement Program
2a	Oak Street Downtown Intersections	Intersection Improvements	a) Implement the Incremental Enhanced Streetscape Alternative at Oak Street/Monroe Street and Oak Street/Polk Street intersections b) Remove traffic signals on Oak Street at Polk and Monroe Streets and provide stop signs c) Replace ADA ramps at Oak Street/Manatee Ave intersection.	City of Arcadia DeSoto County	\$147,861.60 \$3,248.00 \$3,200.00	FDOT LAP City of Arcadia
2b	SR 70 East of Downtown	Complete sidewalk gap on SR 70 East of Downtown	a) Complete sidewalk gap on north side of SR 70 between Roger Ave and 10th Ave	FDOT District 1 Safety Office City of Arcadia	\$8,731.22	Safe Routes to Schools
3	Oak Street Downtown Corridor	Redevelop the streetscape	a) Redevelop Oak St between Desoto Ave and Brevard Ave consistent with the Incremental Enhanced Streetscape Alternative	City of Arcadia	\$394,998.00	FDOT LAP
4	SR 70 & US 17 intersections	Install warning signage	a) Install "Right Turning Vehicles Must Yield to Pedestrians" sign at four intersections of SR 70 and US 17	FDOT District 1 Traffic Operations	N/A	FDOT In-house/Traffic Operations
5	Pasco Avenue Crosswalk Connections	Pasco Ave crosswalk connections	a) Install crosswalk across Oak St to complete path to Memorial Elementary School b) Install ADA ramp at existing crossing location at Pasco Ave and SR 70 EB c) Consider upgrade to Rectangular Rapid Flashing Beacons d) Add sidewalk on Pasco Ave from SR 70 EB to south of Pine Street e) Add sidewalk from Oak Street to SR 70 WB	City of Arcadia	\$64,061.69	Safe Routes to Schools
6	SR 70 Westbound at Mills Ave - Memorial Elementary School	Sidewalk gap and mid-block crossing	a) Complete gap in sidewalks b) Consider upgrade to existing sidewalk to high emphasis with Rectangular Rapid Flashing Beacons c) Consider closing NE Hickory Street between Robert Ave and SR 70	City of Arcadia	\$35,439.56	Safe Routes to Schools
7	US 17 at MLK/Hargrave/Palm St	Sidewalk gap and mid-block crossing	a) Add crossing marking on Palm Street and 1 on MLK/Hargrave b) Add new sidewalk from Palm St to Hibiscus	FDOT District 1 Safety Office City of Arcadia	\$29,151.02	FDOT Planned Capacity Improvement
8	Oak Street West of Downtown Arcadia	Sidewalks and sharrows	a) New sidewalks along Oak St from SR 70 split to Lee Ave b) Review sidewalks from Lee Ave to Manatee Ave for repair/gaps c) Review potential of curb cuts at Dade Ave d) Sharrows along Oak St from SR 70 split to Orange Ave	City of Arcadia DeSoto County	\$74,918.80	FDOT Local Agency Program
9	SR 70 east of Downtown	Extend sidewalk east on SR 70 to Walmart	a) Add sidewalk on SR 70 from Turner Ave/Oak St to east side of Walmart	City of Arcadia DeSoto County	\$107,395.24	3R, LAP, etc.
10	US 17 from Imogene St to Fiveash St	Sidewalk gap	a) Complete gap in sidewalks and add crossings from Imogene St to Fiveash St	FDOT District 1 Safety Office City of Arcadia	\$94,506.00	3R, LAP, etc.
11	SR 70 West of Arcadia at One-Way Pair Split	Mid-block crossing and sidewalk gaps	a) Add crossing markings on SR 70 EB/Oak St EB and WB/SR 70 WB (4-phase crossing) b) Add sidewalk access between Oak Street split and between SR 70 EB and Oak Street c) Add curb extension on SR 70 (WB) d) Add sidewalks on Yule Ave e) Add sharrows on Hickory St from Yule Ave west to terminus at Peace River	FDOT District 1 Safety Office City of Arcadia	\$23,565.56	FDOT Highway Safety Improvement Program
12	SR 70 East of Arcadia at One-Way Pair Split	Mid-block crossing and sidewalk gaps	a) Add crossing markings on SR 70 EB/ SR 70 WB/ across Roger Ave (3 phases) b) Add sidewalks on Oak St from Luther Ave to SR 70 One-Way Pair Split c) Add crossing markings on Oak St at Luther Ave	FDOT District 1 Safety Office City of Arcadia	\$23,121.24	3R, LAP, etc.

Priority #1: Pedestrian crossings on US 17 south of Downtown Arcadia

ISSUE: Lack of infrastructure to support the high volume of pedestrians crossing this section of roadway.

IMPROVEMENT: Add pedestrian crossings at at least two locations—one at Reynolds Street and one at Heard Street. Add crosswalk and add temporary curb refuge island just south of Reynolds Street. Install Rectangular Rapid Flashing Beacons and advanced warning signage.

Table 4: Priority #1 Details

Description	Mid-block crossing
Lead Responsible Agency	FDOT District 1 Safety Office City of Arcadia
Estimated Funding Amount	\$12,429
Potential Funding Source	FDOT Highway Safety Improvement Program

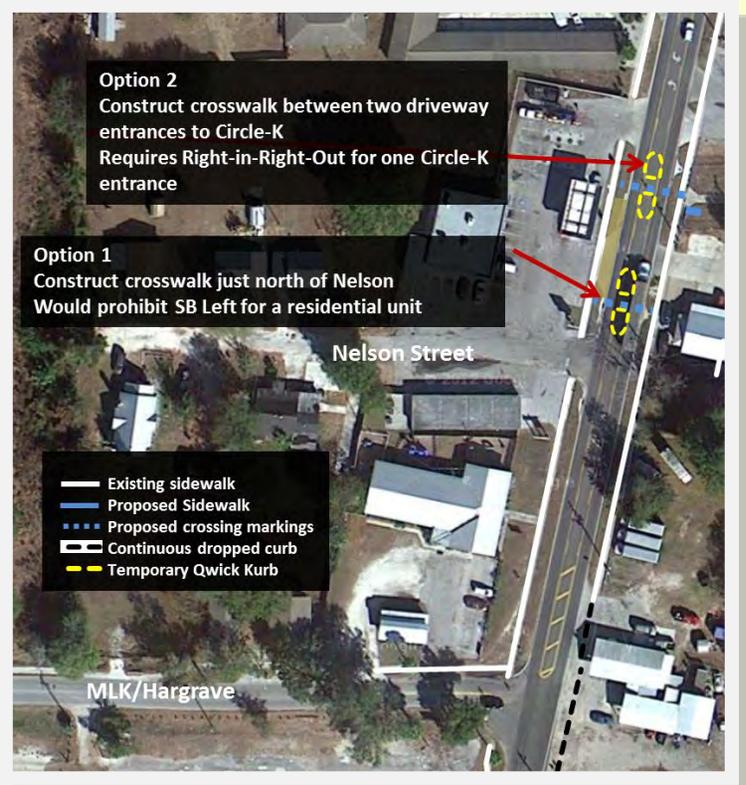
Map 10: Location of Priority #1



Figure 5a: Diagram of Priority #1



Figure 5b: Diagram of Priority #1



Priority #2a: Oak Street Intersections

ISSUE: Lack of infrastructure to support pedestrian and bicycle mobility in downtown. Signals pose unnecessary problem with regard to pedestrian crossing movements and traffic circulation.

IMPROVEMENT: Implement Incremental enhanced Streetscape Alternative to Oak St/Monroe St and Oak St/Polk St intersections. Remove traffic signals at Oak St/Monroe St and Oak St/Polk St Replace ADA ramps at Oak St/Manatee Ave intersection. Replace with stop-control.

Table 5: Priority #2a Details

Description	Intersection Improvements
Lead Responsible Agency	City of Arcadia DeSoto County
Estimated Funding Amount	\$154,309
Potential Funding Source	FDOT Local Agency Program (LAP), City of Arcadia

Map 11: Location of Priority #2a



Figure 6: Image of Priority #2a



Note: Priorities 2a and 2b should be grouped for cost savings.

Priority #2b: SR 70 east of Downtown

ISSUE: Sidewalk gap on north side of SR 70.

IMPROVEMENT: Complete the sidewalk gap on the north side of SR 70 between Roger Ave and 10th Ave.

Table 6: Priority #2b Details

Description	Sidewalk gap
Lead Responsible Agency	FDOT D1 Safety Office, City of Arcadia
Estimated Funding Amount	\$8,731
Potential Funding Source	Transportation Alternatives (Safe Routes to Schools)

Map 12: Location of Priority #2b

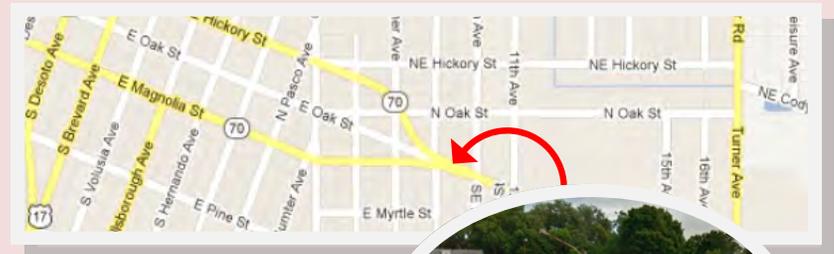


Figure 7: Image of Priority #2b



Priority #3: Redevelop Oak Street between DeSoto Avenue and Brevard Avenue

ISSUE: This block currently functions as a barrier between east and west areas of downtown core due to physical appearance and lack of infrastructure to support bicycle and pedestrian mobility.

IMPROVEMENT: Implement Enhanced Streetscape Alternative to Oak St between DeSoto Ave and Brevard Ave including the intersections.

Figure 8: Diagram of Priority #3

Map 13: Location of Priority #3



Table 7: Priority #3 Details

Description	Redevelop streetscape
Lead Responsible Agency	City of Arcadia DeSoto County
Estimated Funding Amount	\$394,998
Potential Funding Source	FDOT Local Agency Program (LAP)



The Enhanced Streetscape Alternative incorporates key details into a coordinated vision for the redevelopment of Oak Street. The following details are considered to be aligned with the vision for pedestrian and bicycle mobility and an improved physical environment.

STREET TREE LEGEND

- FLOWERING ACCENT TREE
- ☉ PALM TREE
- CANOPY TREE



CANOPY TREE



PALM TREE



VALLEY GUTTER EXAMPLE



FLOWERING ACCENT TREE

Priority #4: Install right-turn warnings at intersections of SR 70 and US 17

ISSUE: Right-turning vehicles may pose a threat to pedestrians crossing at these four intersections.

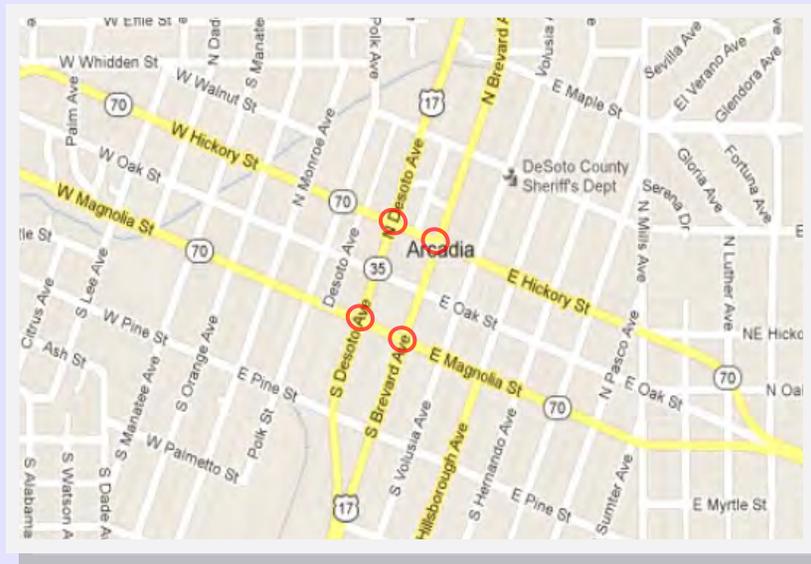
IMPROVEMENT: Install signs warning drivers to “Yield to pedestrians” before turning right.



Table 8: Priority #4 Details

Description	Install warning signage
Lead Responsible Agency	FDOT District 1 Traffic Operations
Estimated Funding Amount	N/A
Potential Funding Source	FDOT District 1 Traffic Operations

Map 14: Location of Priority #4



US 17/DeSoto Ave and SR 70/Hickory Street



Priority #5: Add Pasco Avenue crosswalk connections

ISSUE: Facilitate pedestrian mobility on Pasco Avenue, especially crossing SR 70/Magnolia Avenue.

IMPROVEMENT: Install crosswalk across Oak Street to complete path to Memorial Elementary School. Install ADA ramps at existing crossing locations. Consider upgrade to Rectangular Rapid Flashing Beacons. Add sidewalk from Oak Street to SR 70 Westbound. Add sidewalk from Oak Street to SR 70 Westbound.

Table 9: Priority #5 Details

Description	Crosswalk connections
Lead Responsible Agency	City of Arcadia
Estimated Funding Amount	\$64,061
Potential Funding Source	Transportation Alternative (Safe Routes to Schools)

Map 15: Location of Priority #5



Figure 9: Diagram of Priority #5



Priority #6: SR 70 Westbound at Mills Ave—Memorial Elementary School

ISSUE: Existing gap in sidewalks and lack of an adequate mid-block crossing to support pedestrian crossings.

IMPROVEMENT: Complete gap in sidewalks. Consider upgrade to existing crosswalk to high emphasis with Rectangular Rapid Flashing Beacons. Consider closing NE Hickory Street between Robert Ave and SR 70.

Table 10: Priority #6 Details

Description	Sidewalk gap and mid-block crossing
Lead Responsible Agency	City of Arcadia
Estimated Funding Amount	\$35,439.56
Potential Funding Source	Transportation Alternative (Safe Routes to Schools)

Map 16: Location of Priority #6



Figure 10: Diagram of Priority #6



Priority #7: Extend sidewalk at US 17 south of Palm Street

ISSUE: Lack of sidewalk on west side of US 17 adjacent to mobile home park.

IMPROVEMENT: Complete gap in sidewalks on west side of US 17 from Palm Avenue to Hibiscus Avenue. Add marked crossing on Palm Street.

Map 17: Location of Priority #7

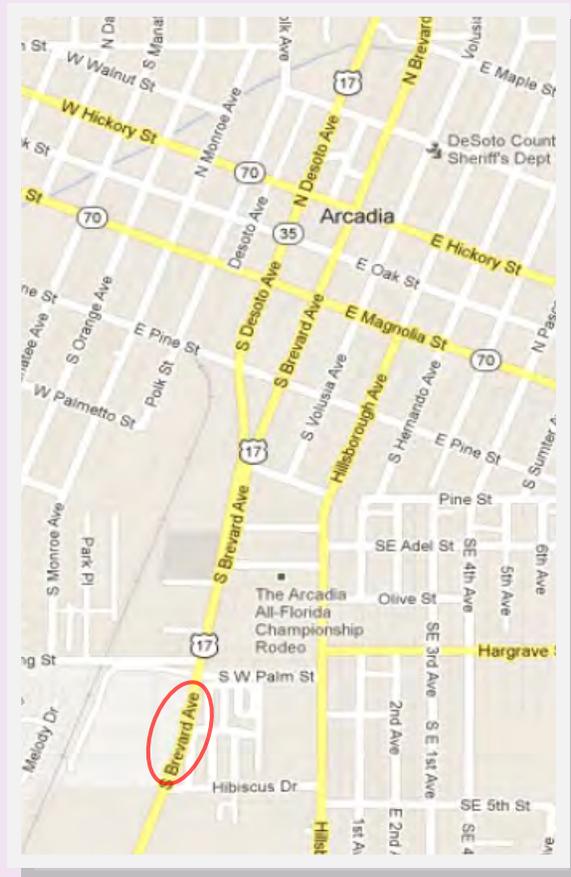


Table 11: Priority #7 Details

Description	Sidewalk extension
Lead Responsible Agency	FDOT District 1 Safety Office, City of Arcadia
Estimated Funding Amount	\$29,151
Potential Funding Source	FDOT Planned Capacity Improvement (No additional action required)

Figure 11: Diagram of Priority #7



Priority #8: Improve Oak Street Downtown connection from SR 70 to Lee Avenue

ISSUE: Lack of connectivity between Downtown and parks on west side of town. Emphasize Oak Street as a pedestrian and bicycle friendly corridor.

IMPROVEMENT: Install new sidewalks along Oak Street from SR 70 split to Lee Avenue. Review sidewalks from Lee Avenue to Manatee Avenue for repair/gaps. Review potential of curb cuts at Dade Avenue. Add sharrows along Oak Street from SR 70 split to Orange Avenue.

Figure 12: Diagram of Priority #8

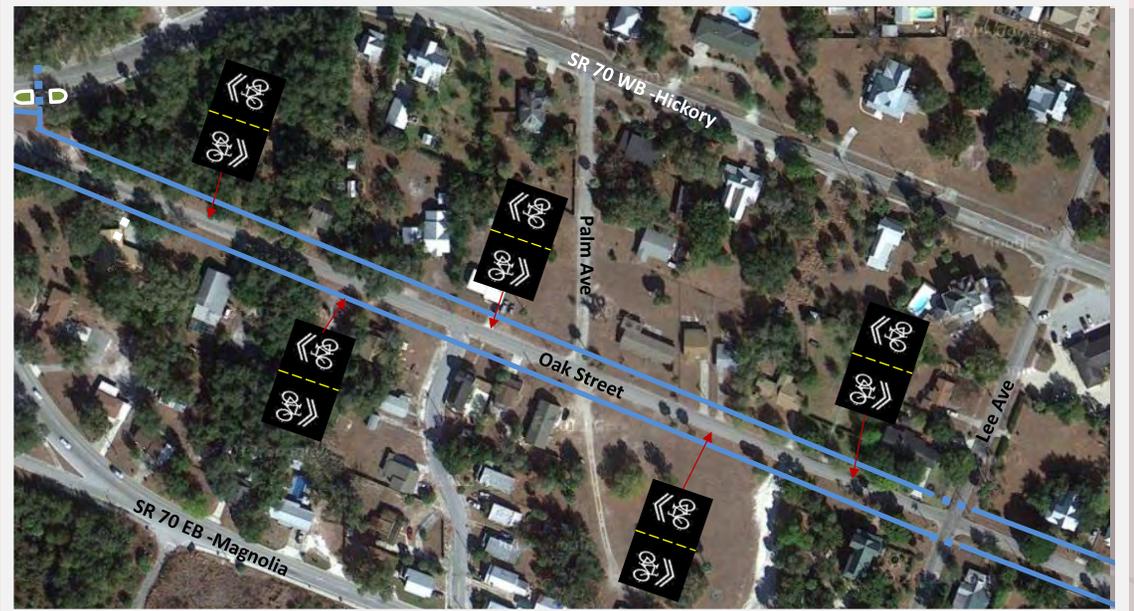
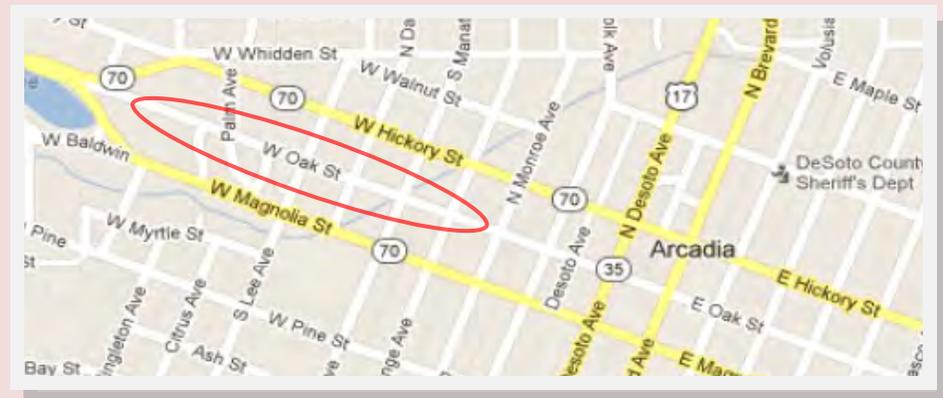


Table 12: Priority #8 Details

Description	Sidewalks and Sharrows
Lead Responsible Agency	City of Arcadia DeSoto County
Estimated Funding Amount	\$74,918
Potential Funding Source	FDOT Local Agency Program (LAP)

Map 18: Location of Priority #8



Priority #9: SR 70—Extend Sidewalk to Walmart

ISSUE: Lack of connectivity between Downtown and major commercial destination.

IMPROVEMENT: Install new sidewalks along the south side of SR 70 from Turner Avenue to Walmart.

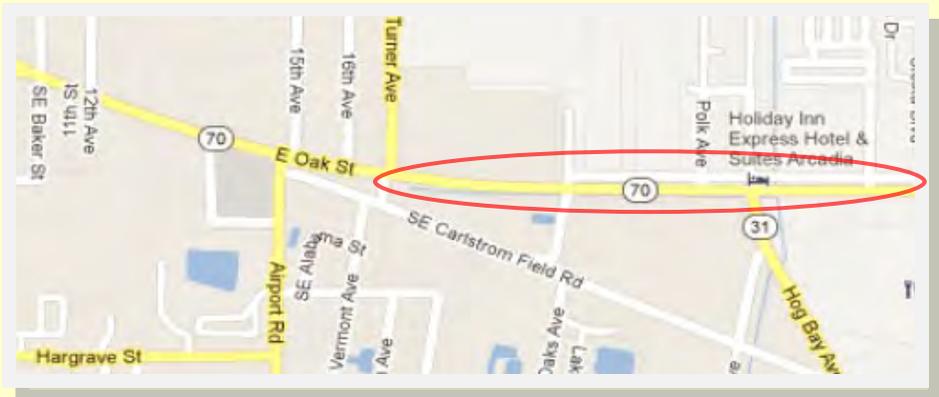
Figure 13: Diagram of Priority #9



Table 13: Priority #9 Details

Description	Sidewalk extension
Lead Responsible Agency	FDOT District 1 Safety Office City of Arcadia, DeSoto County
Estimated Funding Amount	\$107,395
Potential Funding Source	Resurfacing, Rehabilitation, Reconstruction (3R), Local Agency Program (LAP), etc.

Map 19: Location of Priority #9



Priority #10: US 17 from Imogene Street to Fiveash Street

ISSUE: Lack of connectivity between Downtown and major hospital.

IMPROVEMENT: Install new sidewalks along the East side of Brevard Avenue from Imogene Street to Fiveash Street.

Table 14: Priority #10 Details

Description	Sidewalk gap
Lead Responsible Agency	FDOT District 1 Safety Office City of Arcadia, DeSoto County
Estimated Funding Amount	\$94,506
Potential Funding Source	FDOT Local Agency Program (LAP)

Figure 14: Diagram of Priority #10



Priority #12: Add pedestrian crossings on SR 70 east of Downtown

ISSUE: Lacking a safe place to cross the street.
Gaps in existing sidewalk are hazardous.

IMPROVEMENT: Add crossing markings on SR 70 westbound across Roger Avenue (3 phases). Add sidewalks on Oak Street from Luther Avenue to SR 70 one-way pair split. Add crossing markings on Oak Street at Luther Avenue.

Table 16: Priority #12 Details

Description	Midblock crossings, sidewalk gaps
Lead Responsible Agency	FDOT District 1 Safety Office City of Arcadia, DeSoto County
Estimated Funding Amount	\$23,121,24
Potential Funding Source	Resurfacing, Rehabilitation, Reconstruction (3R), Local Agency Program (LAP), etc.

Map 21: Priority #12



Figure 16: Diagram of Priority #12



Additional Action Items

In addition to the priorities identified on the previous pages, the following general Action Items were identified throughout the planning process.

1. **Coordinate safety outreach/education and enforcement.**

A coordinated and sustained local educational outreach program was identified as being a key opportunity to support infrastructure improvements. Concurrently, law enforcement should be actively involved in the safety message as well as supportive in complementing the education efforts with enforcement activities. This can include safety activities and programs in schools using the resources at the state local and state level.

Potential lead agencies: DeSoto County Health Department, FDOT District 1 Community Traffic Safety Team

2. **Convert existing alleyways in Downtown into pedestrian areas.**

The need for more pedestrian-oriented areas downtown was cited as a key concern in the public workshops, such as outdoor seating areas with café tables with chairs. These types of improvements contribute to preserving a sense of place and improve the appearance and value of the city. Implementing these improvements in portions of the existing alleyway spaces throughout Downtown is an excellent way to improve continuity and create a better pedestrian experience. Additionally, these spaces are already public rights-of-way and would require minimal maintenance.

Potential lead agencies: City of Arcadia, Arcadia Main Street

3. **Maintain momentum with addressing safety problems and high-crash locations.**

Maintain the momentum that this plan creates by focusing on identifying key safety concerns where pedestrian or bicycle accidents are occurring. These issues should be openly discussed, investigated, and followed through by tapping into safety resources such as the FDOT Highway Safety Improvement Program and Local Agency Program.

Potential lead agencies: City of Arcadia, DeSoto County, FDOT District 1 Community Traffic Safety Team

Conclusion

The City of Arcadia Bicycle and Pedestrian Master Plan relies upon the endorsement of stakeholders and citizens in support of the improvements and action items that have been identified in this report. Moving forward, the development and implementation of the priority improvements identified in this report will be overseen by the identified lead responsible agency. These stakeholder agencies—the Florida Department of Transportation District 1 Traffic Operations/Safety Office, DeSoto County, the City of Arcadia, Arcadia Main Street, and the local Community Traffic Safety Team—will rely on continued communication and coordination in the implementation of the priority improvements and fulfillment of the additional action items.





For additional information on the City of Arcadia Bicycle and Pedestrian Master Plan,
contact Jared Schneider at JSchneider@findaleoliver.com or (813) 224-8862.